

Reason:

Championships and events can currently apply for authorisation to use FIA specification fuel which is granted without a qualifying criteria. Including reference to FIA Appendix J Art 252 fuel specification in (B) Pump Fuel allows events & Championships to use FIA specification fuel without specific approval. A revised FIA fuel specification is to be applied from 1st January 2018 so this proposal is timed to coincide with this new specification.

The retention of the MSA Specification in (B) Pump Fuel retains the ability for events/championships to specify the current 100-RON limit if they wish as the regulatory principle that restrictions can be tightened but not opened applies.

Provision has also been made for Karting to use CIK specification fuel without the need for individual approval.

We have also taken this opportunity to tidy up the reference to LRG which was unclear in the existing regulation.

(K) Competitors: Safety

Date of implementation: 1 January 2019

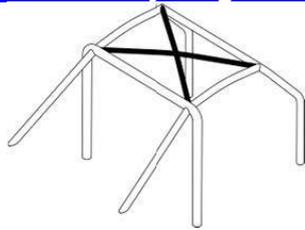
1.2.2. The ROPS must not unduly impede Driver and Co-Driver access to the vehicle. Members may intrude into the interior space by passing through the dashboard, front side trim, rear side trim, and rear seats which may be folded down. ROPS must not extend beyond the front upper or rear suspension mounting points of the vehicle, with the exception of backstays which may extend beyond the rear suspension mounting points if necessary to achieve the required 30° angle. Any modification to a homologated ROPS (other than fitment of **Roof Reinforcement, Windscreen Pillar Reinforcement, Doorbars and Harness Bars**) is prohibited. Tubes must not carry any fluids.

Reason: To mandate the use of roof reinforcement bars within Stage Rally vehicles, which help prevent intrusion from the roof panel. Additionally, due to the shallow angle of modern car windscreens becoming a vulnerable area, further reinforcement should be mandated to the windscreen pillars.

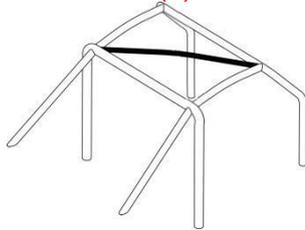
Existing vehicles competing prior to 1st January 2019 may comply with the following until 1st January 2022. New build vehicles from 1st January 2019 must comply with Appendix 3.

3. A fire extinguisher/extinguishing system must be carried on all vehicles, the minimum requirement being that the system be charged with one of the permitted extinguishants and be operable by the driver whilst normally seated either by manual operation or by a mechanically/electrically assisted triggering system.

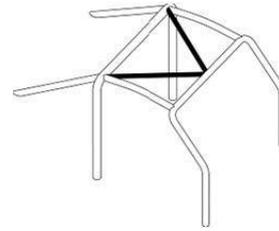
Appendix 2: Safety Cage Drawings



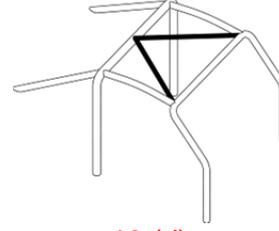
10 (a)



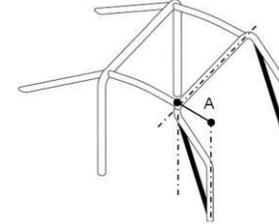
10(b) Bar may be fitted in either orientation.



10 (c)

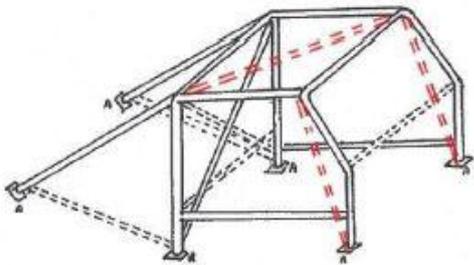


10 (d)

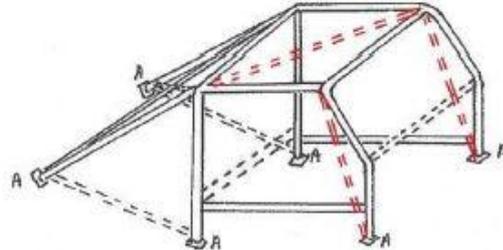


63

a) Diagonal(s) across Main Hoop



b) Diagonal(s) across Back Stays



==== Mandatory minimum requirement

----- Maximum options permitted

A Mandatory mounting points

Additionally a further 8 points may be attached to the bodyshell.

Competitors should note that an installation to this drawing may contravene FIA Appendix K regulations.

Doorbars may alternatively be as shown in figure 12(g) or (h). **Roof Reinforcement may alternatively be as shown in figure 10(a) to 10(d)**

Drawing No. 37

Reason: As a result of Rallies Committee's proposal to require Roof Reinforcement and Windscreen Pillar Reinforcement. Drawing No. 37 needs to be amended to allow these additional ROPS bars in a historic stage rally vehicle.

Over 1150cc and up to and including 1600 cc: 8in
Over 1600 cc: 9in.

Reason: Many production cars under 1150cc are fitted wider wheel tyres combinations as OE.

It is not always possible to source a narrower combination without reducing rim diameter which in turn may foul the brakes.

Date of implementation:
1 January 2019

48.10.1. Have, as a minimum, safety roll-over bars complying with K.1, Section K Appendix 2 Drawing number 5 or 6, and longitudinal door bars complying with K.1.3.5(b) (side sections K Appendix 2 Drawing number 9).

For vehicles first issued with a Vehicle Passport from 1st January 2019 the following additional members are mandatory:

a) Roof Reinforcement complying with K.1.3.5 (c) and Section K Appendix 2 drawing number 10 (a), (b), (c) or (d).

b) Windscreen Pillar Reinforcement complying with K.1.3.5 (e) on each side if Section K Appendix 2 drawing number 63 dimension "A" is greater than 200 mm.

Reason: To mandate the use of roof reinforcement bars within Stage

Rally vehicles, which help prevent intrusion from the roof panel. Additionally, due to the shallow

angle of modern car windscreens becoming a vulnerable area, further reinforcement should be mandated to the windscreen pillars.

(S) Sprints, Hill Climbs and Drag Racing

Date of implementation:
1 January 2018

7.1.6. Competitors must declare on their entry forms whether or not they have ever held a valid RTA licence. Competitors who have never held a valid RTA licence and who do not have any qualifying signatures towards upgrading their Competition Licence on their Upgrade Card may be specifically observed at the event.

(a) Any competitor whose vehicle is not self starting, at any part of the course, must declare on their entry form the method of operation together with a detailed starting instruction.

Reason: Safety. To allow for assessment risk by the Clerk of the Course in preparation of the meeting as well as knowledge where a vehicle may stop on track.

9.3.1.1. With the exception of Drag Racing, the heating of tyres, by any method other than 9.3.1. above, or their treatment by any chemical substance is prohibited. For the avoidance of doubt, no manner of abnormally raising the temperature of the tyres/wheels above the natural ambient temperature is allowed.